

# Leveraging Grants for Port Infrastructure

CITY OF

*Aberdeen*

A GREAT PLACE TO LIVE, WORK & PLAY



# Background





2003



2007

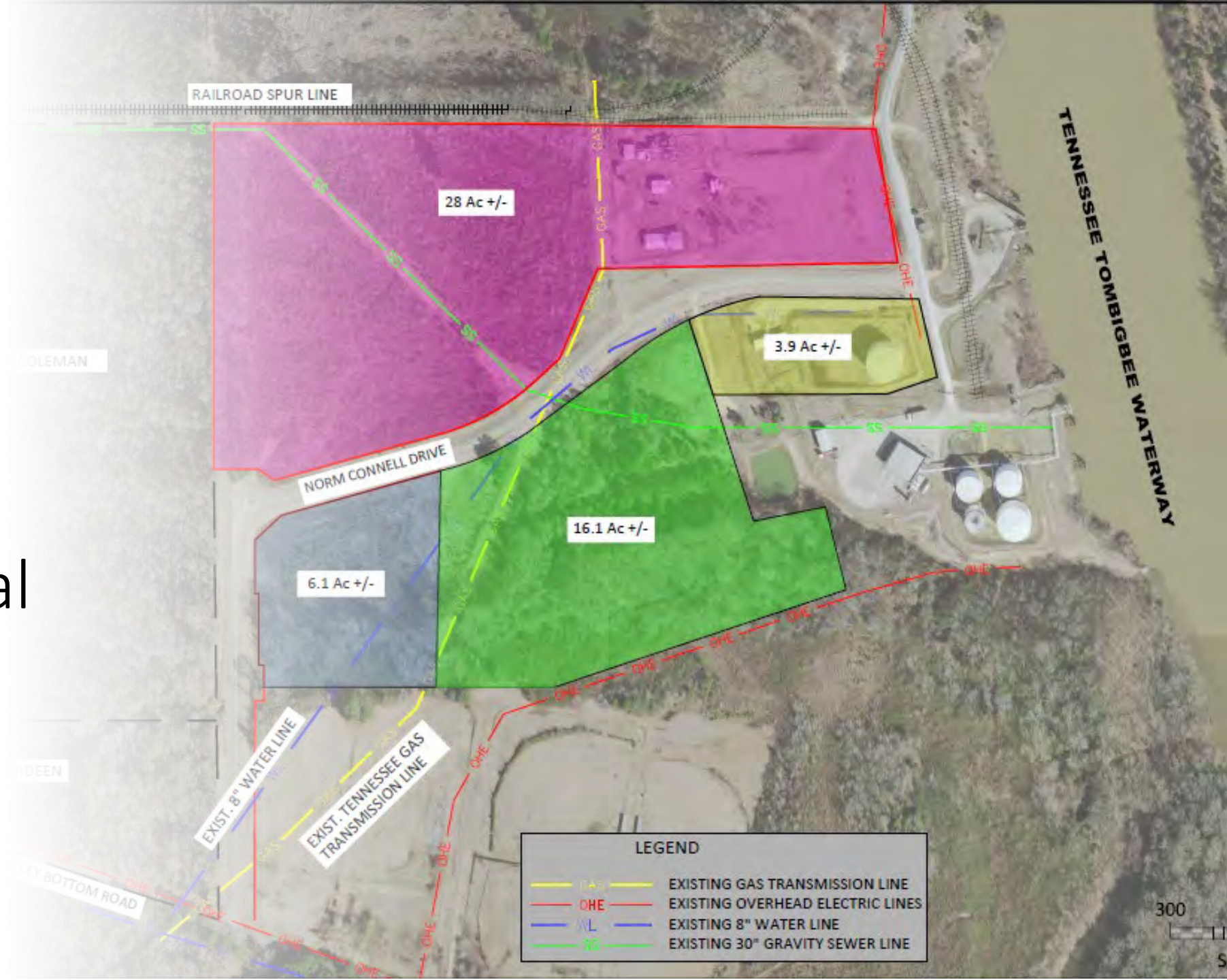


2013



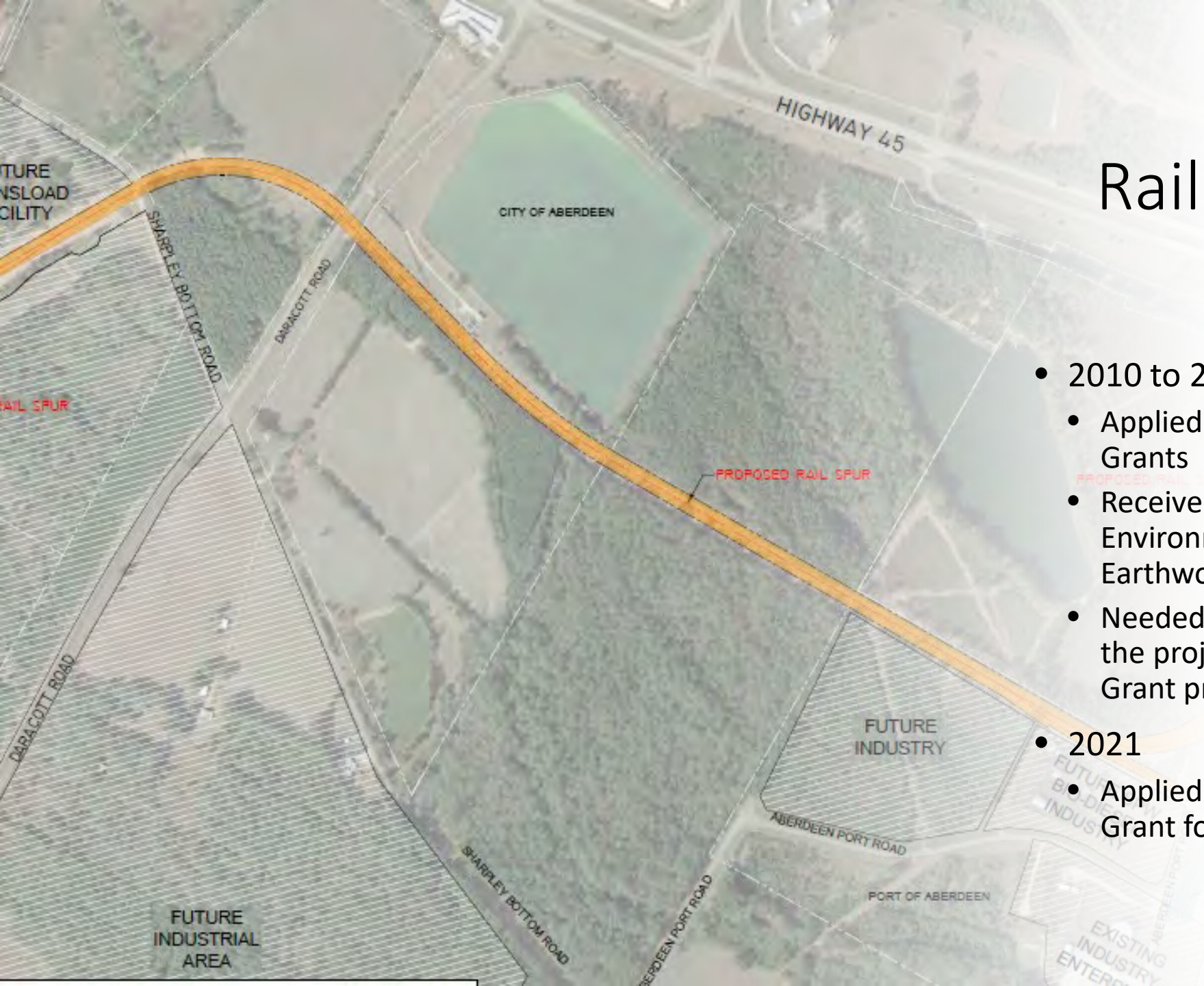
2016

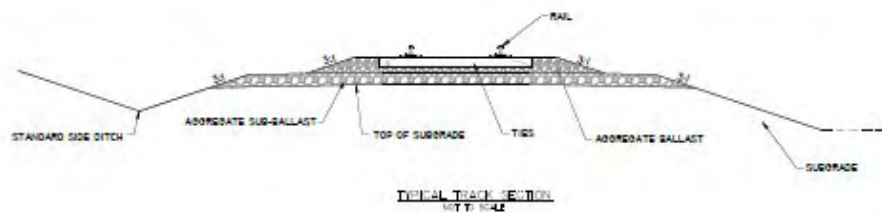
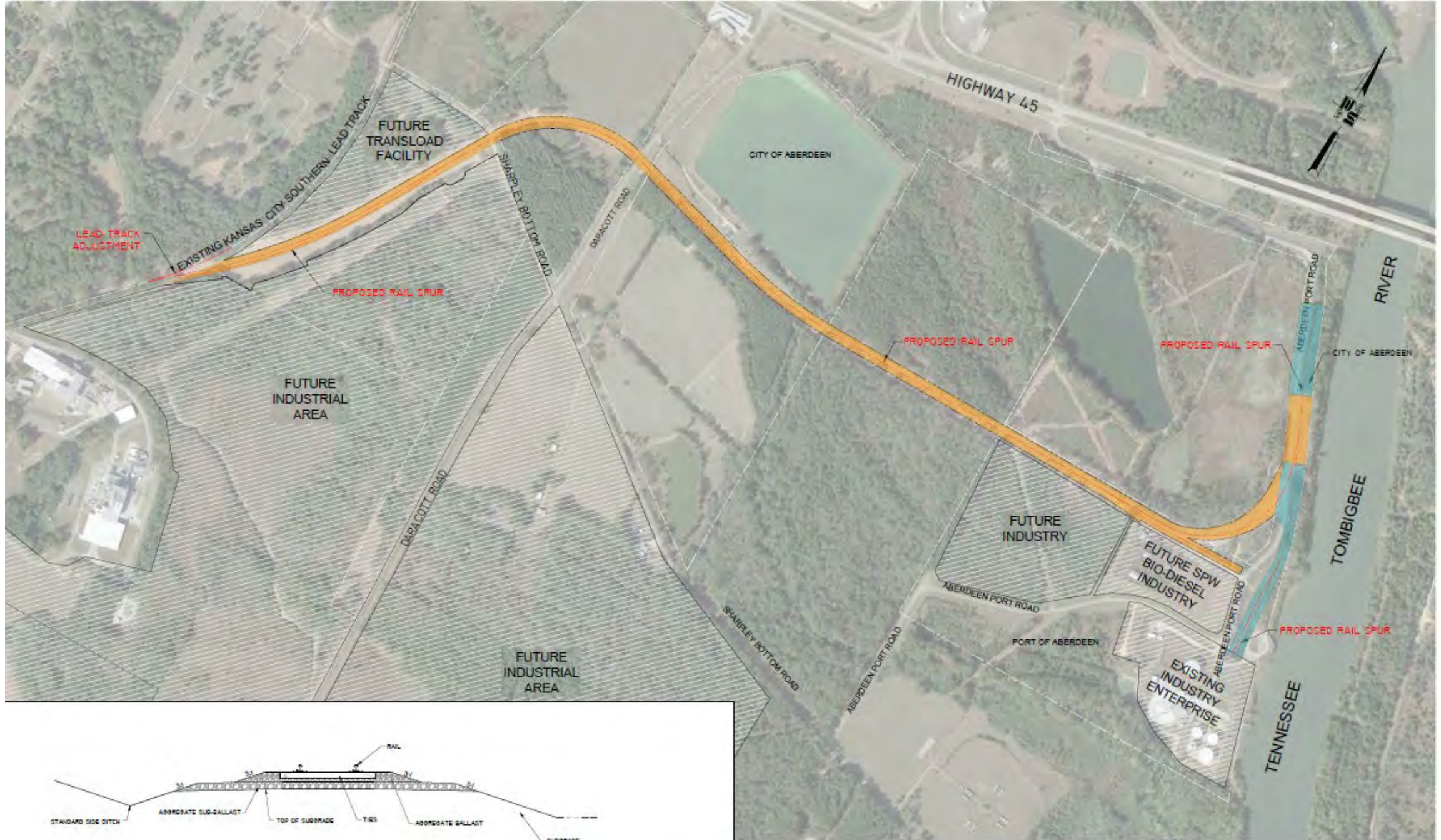
# Port Potential



# Rail Spur Project

- 2010 to 2017
  - Applied for numerous MDOT Multi Modal Grants
  - Received a total of \$1.2 million for ROW, Environmental, Wetland Mitigation, Clearing, Earthwork, Drainage, and Utility Relocations.
  - Needed additional \$4 million to complete the project. Too large for MDOT Multi Modal Grant program.
- 2021
  - Applied for and won FY '21 MARAD PIDP Grant for \$4 million to complete the rail spur.





SUBGRADE COMPLETE ..... ———  
 CLEARING & GRUBBING REQUIRED ..... ———

An aerial photograph of a port dock facility. A large, circular concrete structure, possibly a fender or a small island, is situated in the water. To the left of this structure is a paved road with a white vehicle parked on it. The surrounding area is a mix of dirt, grass, and some sparse vegetation. The water is a murky green color.

# Port Dock Facilities Upgrades

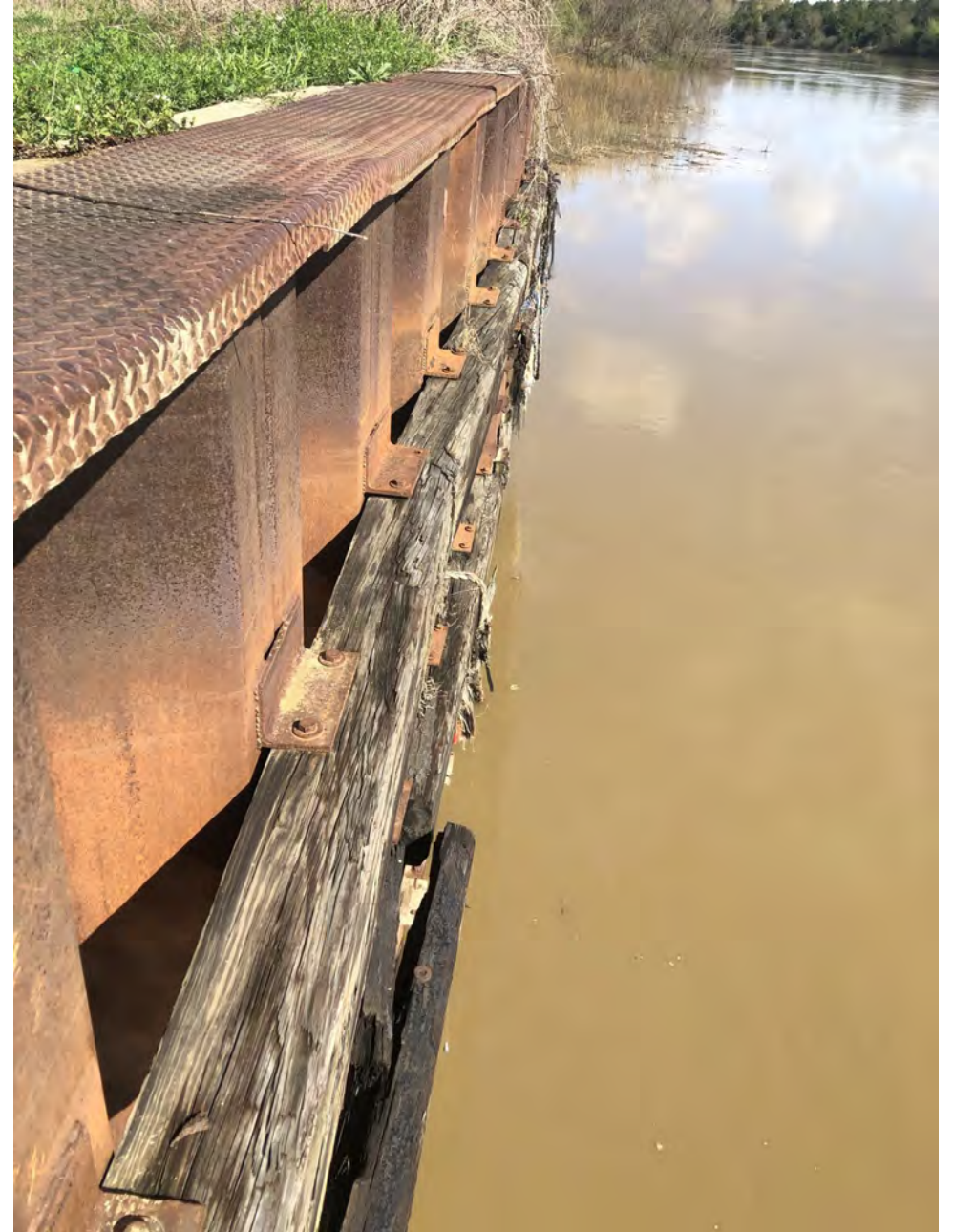
- Applied for and won FY '22 MDOT Multi Modal Grant for \$450,000 to rehab/upgrade port facilities.
  - Demo dilapidated scale house and block building
  - Demo concrete retaining wall and asphalt
  - Remove/abandon steel hopper
  - Install new handrail and ladder on dock wall
  - Install new rubber tire fender system on dock wall











**GENERAL:**

1. GENERAL BUILDING CODE: INTERNATIONAL BUILDING CODE, 2015 EDITION.

2. CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND SITE CONDITIONS PRIOR TO STEEL FABRICATION AND CONSTRUCTION AND SHALL NOTIFY THE ENGINEER IF ANY DISCREPANCIES ARE NOTED.

1. STRUCTURAL STEEL DESIGN CODE: AISC SPECIFICATION FOR STRUCTURAL STEEL BUILDINGS, AISC MANUAL OF STEEL CONSTRUCTION 14TH EDITION.

2. ALL STRUCTURAL STEEL PLATES, ANGLES, AND CHANNELS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A36.

3. ALL DETAILING, FABRICATION AND ERECTION OF STRUCTURAL STEEL, UNLESS OTHERWISE NOTED, SHALL CONFORM TO THE REQUIREMENTS OF THE AISC SPECIFICATIONS FOR BUILDINGS, LATEST EDITION.

4. WELDS SHALL BE MADE WITH E-70XX ELECTRODES.

5. UNLESS OTHERWISE NOTED, ALL FIELD CONNECTIONS SHALL BE MADE WITH 3/4" DIA HIGH STRENGTH BOLTS (ASTM A-325). CONNECTIONS SHALL BE DESIGNED AS BEARING TYPE WITH THREADS IN SHEAR PLANE. ALL A-325 BOLTS SHALL BE INSTALLED USING THE TURN OF THE NUT METHOD AS SPECIFIED IN THE MANUAL OF STEEL CONSTRUCTION, 14TH EDITION.

1. ALL PILES OR "W" SHAPES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A572 Gr. 50 (Fy=50 KSI).

3. REFERENCE DETAIL 4/S1.03 FOR TYPICAL PILE SPLICE DETAIL.

4. ALL PILES (DOLPHINS ONLY) SHALL SAND BLASTED TO PREPARE SURFACES FOR PAINTING.

5. ALL PILES (DOLPHINS ONLY) SHALL HAVE TWO COATS OF COAL TAR EPOXY WITH A FINISHED DRY THICKNESS OF 16mils MIN.

1. CONCRETE DESIGN CODE: BUILDING CODE REQUIREMENTS FOR STRUCTURAL CONCRETE - ACI 318-14.

2. ALL CONCRETE SHALL CONFORM TO THE REQUIREMENTS OF THE ALABAMA DEPARTMENT OF TRANSPORTATION STANDARDS SPECIFICATIONS, 2012 EDITION, FOR BRIDGE SUBSTRUCTURE CONCRETE, CLASS A.

3. UNLESS OTHERWISE NOTED, ALL REINFORCING STEEL SHALL BE GRADE 60, DEFORMED BARS, CONFORMING TO ASTM A615.

4. UNLESS OTHERWISE NOTED, ALL DETAILING, FABRICATION AND PLACING OF REINFORCING STEEL SHALL CONFORM TO THE MANUAL OF STANDARD PRACTICE FOR DETAILING REINFORCED CONCRETE STRUCTURES (ACI-SP-66-LATEST EDITION).

5. ALL BAR SPLICES SHALL BE CLASS 'B' TENSION SPLICES, AS SPECIFIED IN ACI 318-14, UNLESS OTHERWISE NOTED. REINFORCEMENT SHALL NOT BE WELDED UNLESS APPROVED BY THE ENGINEER.

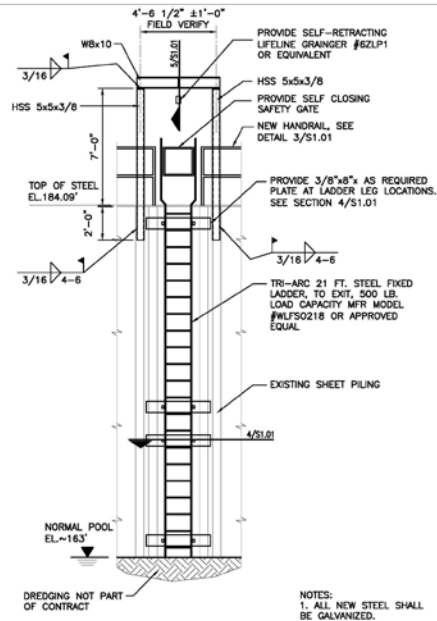
6. ALL CONCRETE SHALL BE CURED USING WET METHODS OR CURING COMPOUND PER ACI 301.

	$f_c=4000 \text{ psi}$
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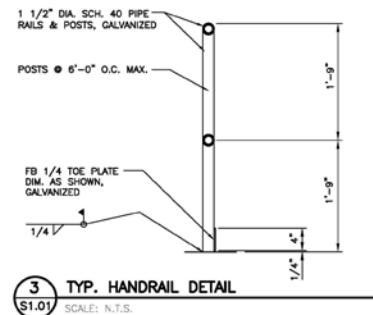
SIZE	TOP BARS	OTHER BARS
	B	B
#3	25"	19"
#4	33"	25"
#5	41"	31"
#6	49"	37"
#7	71"	54"
#8	81"	62"
#9	91"	70"
#10	102"	79"
#11	114"	87"

**NOTE:**

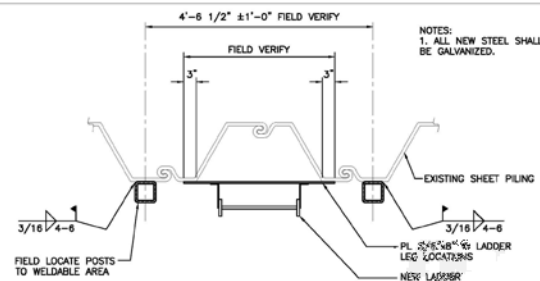
2. ABOVE CHART IS APPLICABLE TO GRADE 60 REINFORCEMENT.
2. TOP BARS ARE HORIZONTAL BARS WITH MORE THAN 12" OF FRESH CAST BELOW THE BAR.
3. WHEN BAR COVER IS EQUAL TO OR LESS THAN THE BAR DIAMETER OR THE BAR SPACING IS TWICE THE BAR DIAMETER OR LESS, SPLICE LENGTHS SHALL BE INCREASED BY 100%.



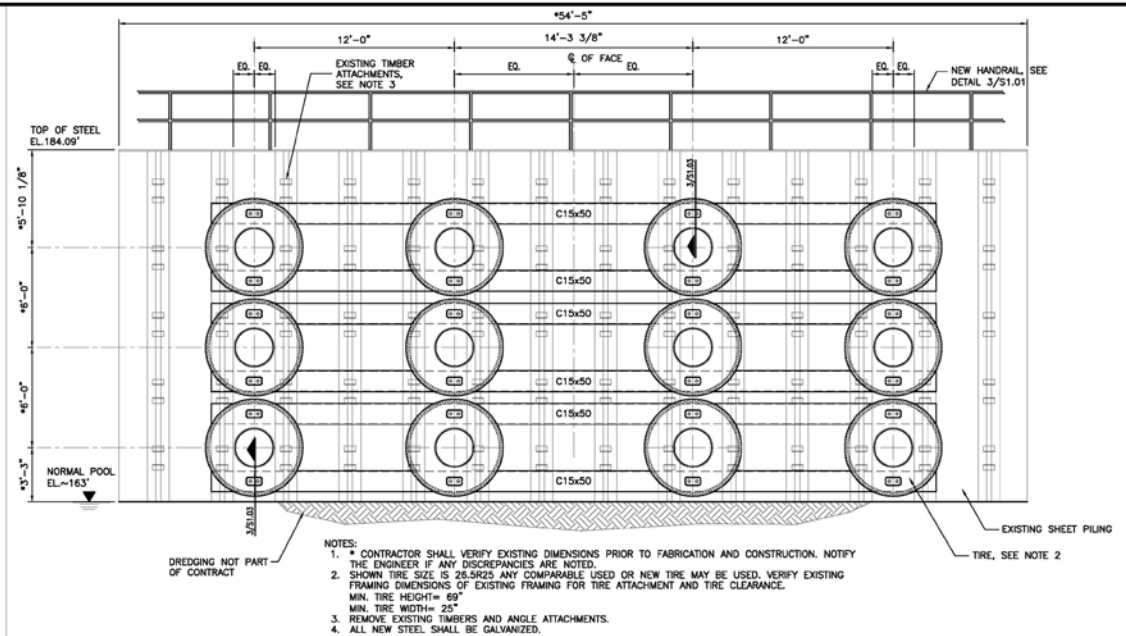
2 SCHEMATIC LADDER ELEVATION  
S1.01 SCALE: N.T.S.



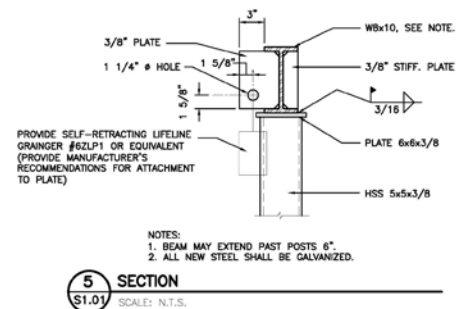
3 TYP. HANDRAIL DETAIL  
S1.01 SCALE: N.T.S.



**4** SECTION  
S1.01 SCALE: N.T.S.



**1 SCHEMATIC DOCK ELEVATION**  
S1.01 SCALE: N.T.S.



5 SECTION  
S1.01 SCALE: N.T.S.





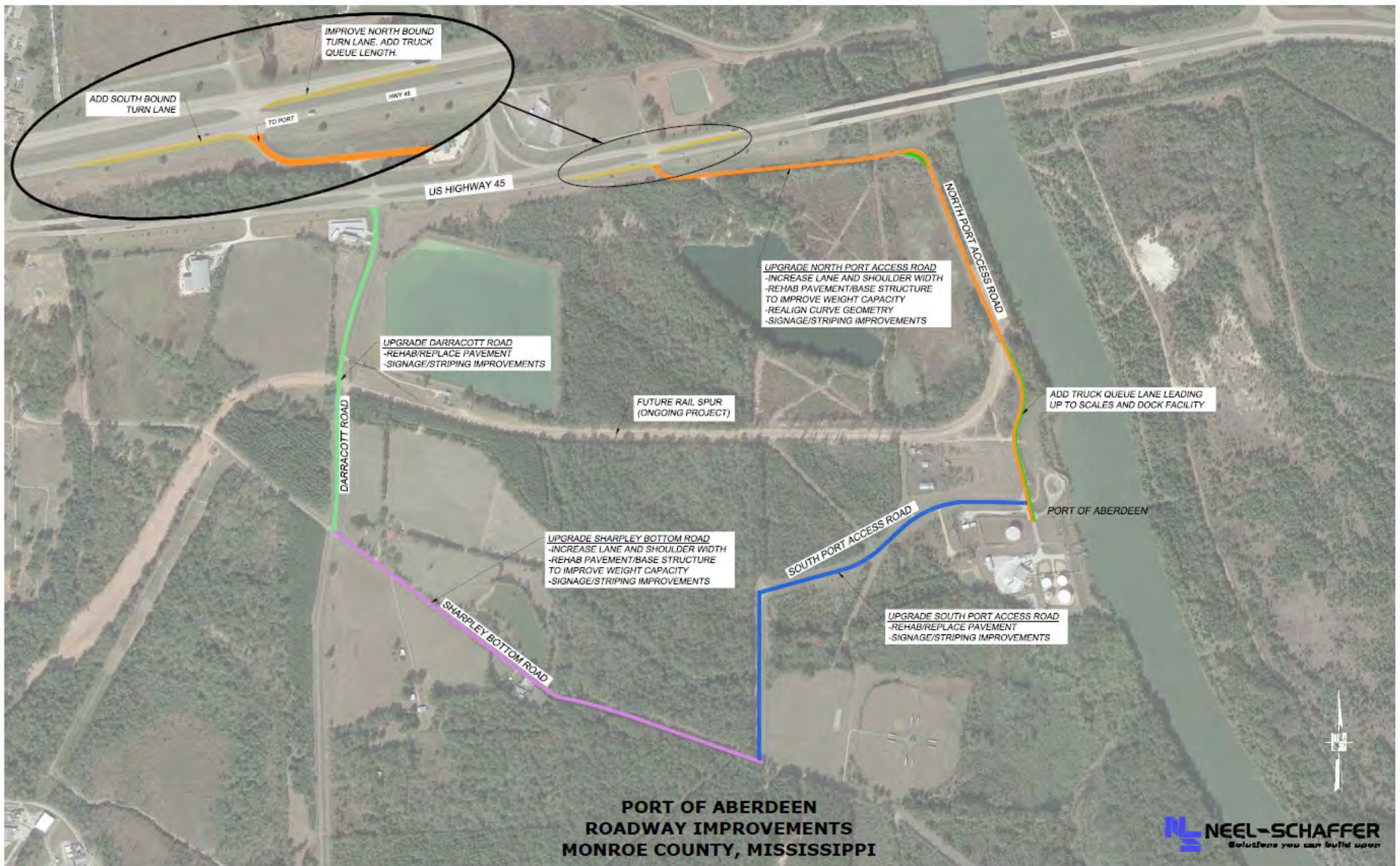




# What's Next?

- Roadway Improvements - \$4.5 Million (Applied for FY '22 MARAD PIDP & RURAL Surface Transportation Grant)
- Remaining Port Dock Facility Upgrades - \$900k
- Dredging - \$400k (Port Maintenance Fund)
- Operations Equipment - \$2.5 Million
- Office/Shop/Storage Space - \$750k





**PORT OF ABERDEEN  
ROADWAY IMPROVEMENTS  
MONROE COUNTY, MISSISSIPPI**